

## Connectivity & Accessibility

Connectivity and accessibility are two of the major transportation-related issues that impact all demographics in Vinton, as shown on the Transportation Assets and Barriers Analysis boards (see 3a-3c). Inadequate sidewalk infrastructure such as incomplete, broken, narrow, and rising sidewalks and inconsistent curb ramps, threatens user safety and limits pedestrian mobility. It also portrays a less than desirable view of the community to passersby.

Citizen focus groups identified the need to improve the sidewalk system, which was further echoed by community members participating in the public design workshop held on June 29, 2023, during the Vinton Farmers Market. Desired improvements included making the sidewalks wider, accessible and ADA compliant, complete, and connected to key destinations throughout town - refer to Board 3c.

### Pedestrian Circulation

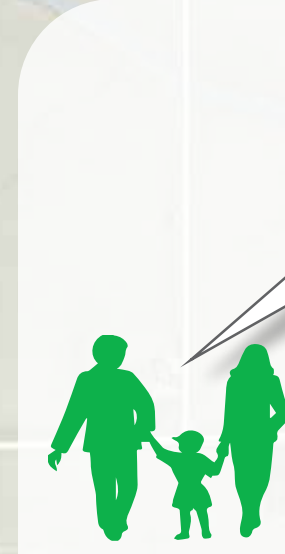
The map on this board illustrates the input given by the community members and steering committee during the public design workshop, as well as that of the focus groups. In addition to an improved sidewalk system, the community would like to see an expanded trail system, designated safe routes to school, designated high-visibility crosswalks, and shared roads, as appropriate.

While the goal is to have all residential, commercial, and public areas served by an ADA-compliant sidewalk that is in good condition, the map on this board identifies the highest priority areas.

"[The] sidewalks are horrible...all over...That's why a lot of people walk in the street...That's the only place you can walk."



Older Adults



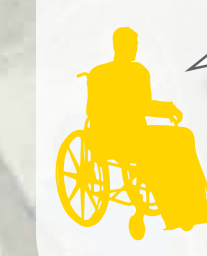
"I would like...[a] biking path for kids to be able to ride their bikes on for a significant amount of time and be safe the entire time..."

Parents



Actives

"I love the idea of connecting the Old Creamery [Trail] to the Glow Trail..."



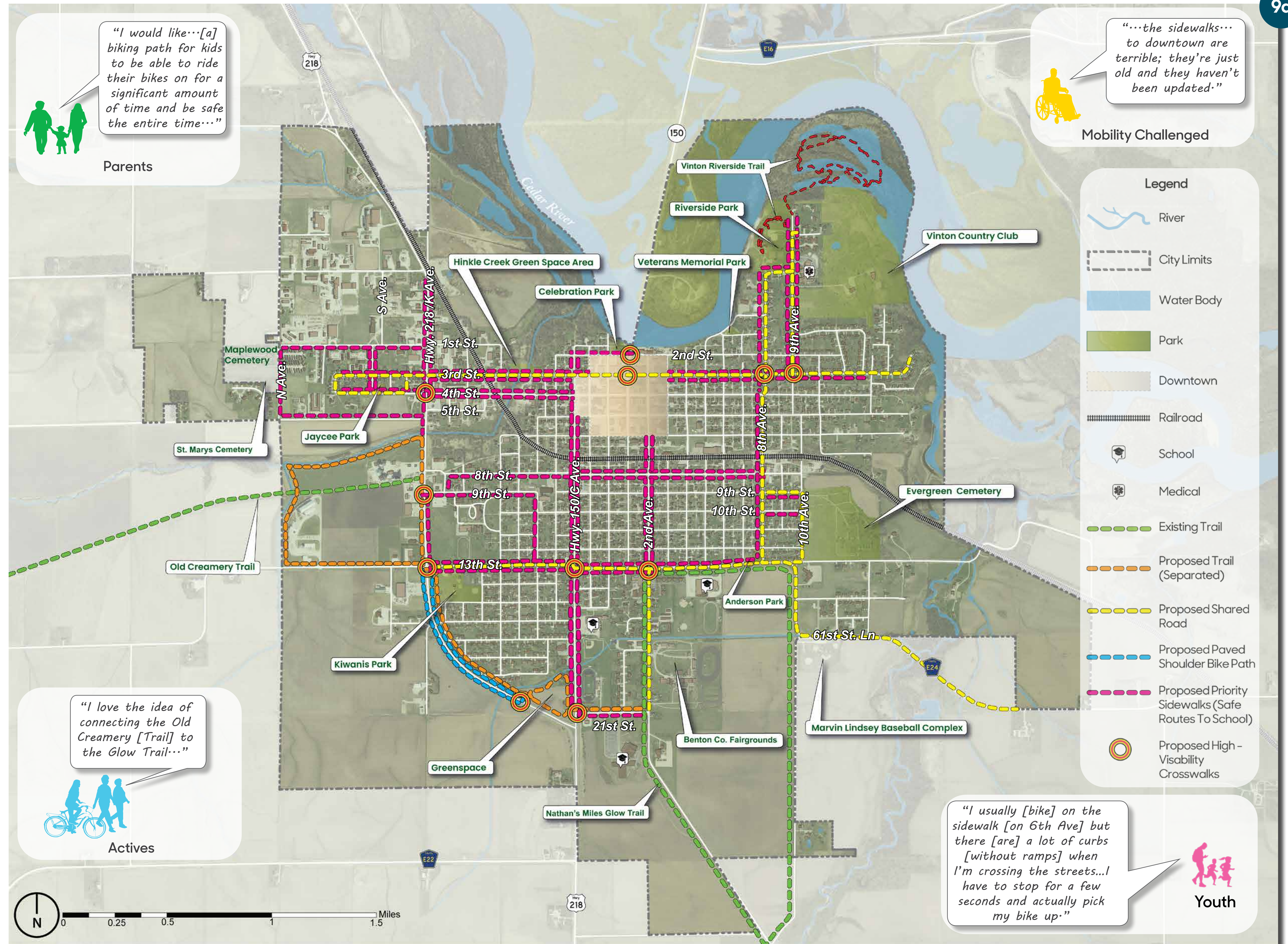
"...the sidewalks... to downtown are terrible; they're just old and they haven't been updated."

Mobility Challenged

"I usually [bike] on the sidewalk [on 6th Ave] but there [are] a lot of curbs [without ramps] when I'm crossing the streets...I have to stop for a few seconds and actually pick my bike up."



Youth



This map is the pedestrian circulation concept plan. It highlights existing trails, proposed trail extensions (separated, shared road, and shoulder) as well as priority sidewalks. The priority sidewalks are to be complete, meet ADA accessibility requirements and serve as Vinton's "Safe Routes To School."



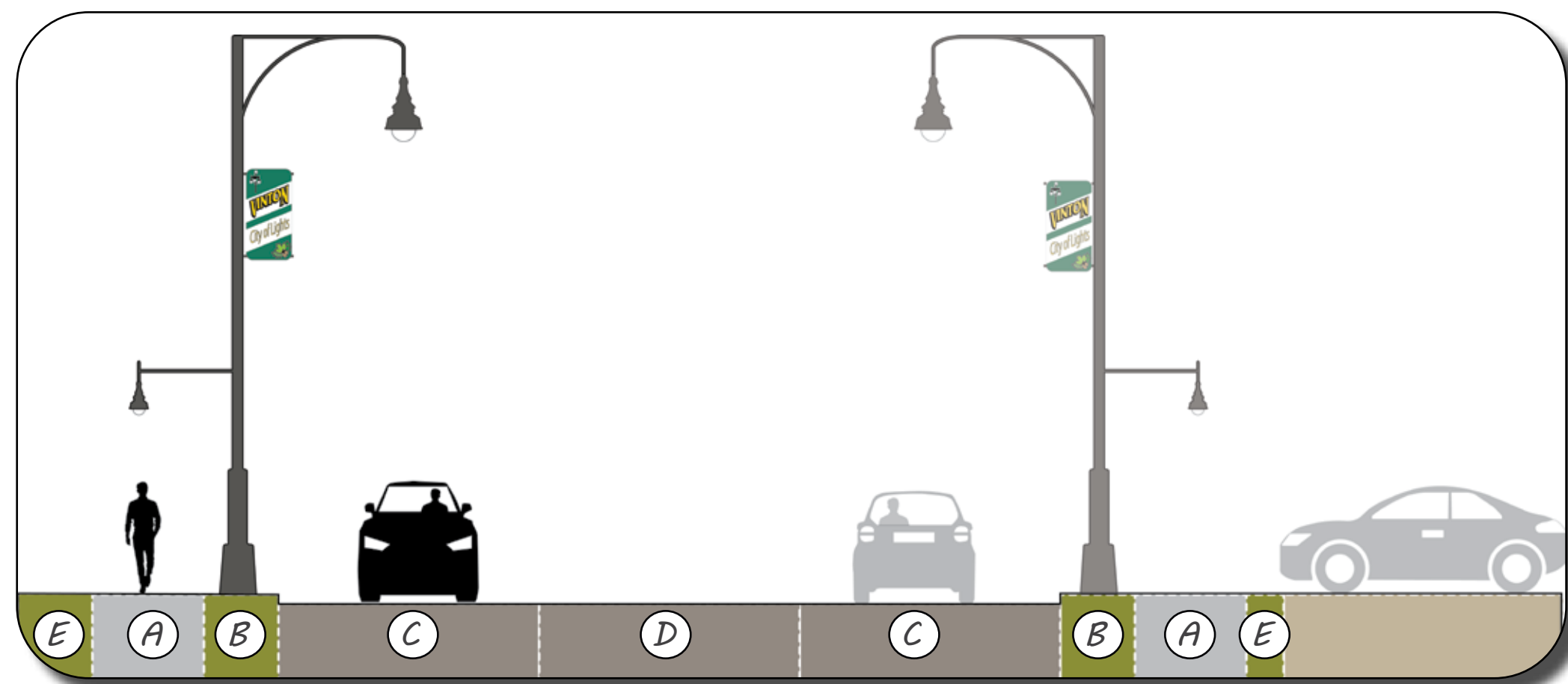
## Highway 218 (K Avenue)

Residents identified the lack of pedestrian connectivity and accessibility along state Highway 218 as a major transportation barrier during the preliminary planning process - see boards 2 and 4. Heavily traveled, Hwy. 218 is one of the main arterials for residents and visitors, and provides the first impression of Vinton to travelers entering town from the northwest and south.

The proposed enhancements illustrated on this board address the transportation barriers noted by residents for Hwy. 218 as well as various other transportation-related concerns. The enhancements are targeted at turning Hwy. 218 into a "complete street," while also improving circulation and augmenting the streetscape. The same strategies shown on these boards can be applied to other primary corridors as noted on board 5, including 4th Street.

*"...there's no sidewalk on [the Highway] 218...bridge [over Hinkle Creek]...it's very, very narrow...that becomes a safety hazard."*

*"There's a bridge [on Hwy 218], but there [are] no sidewalks... people [are] walking in the bridge as people are driving over it. I've almost hit people there."*



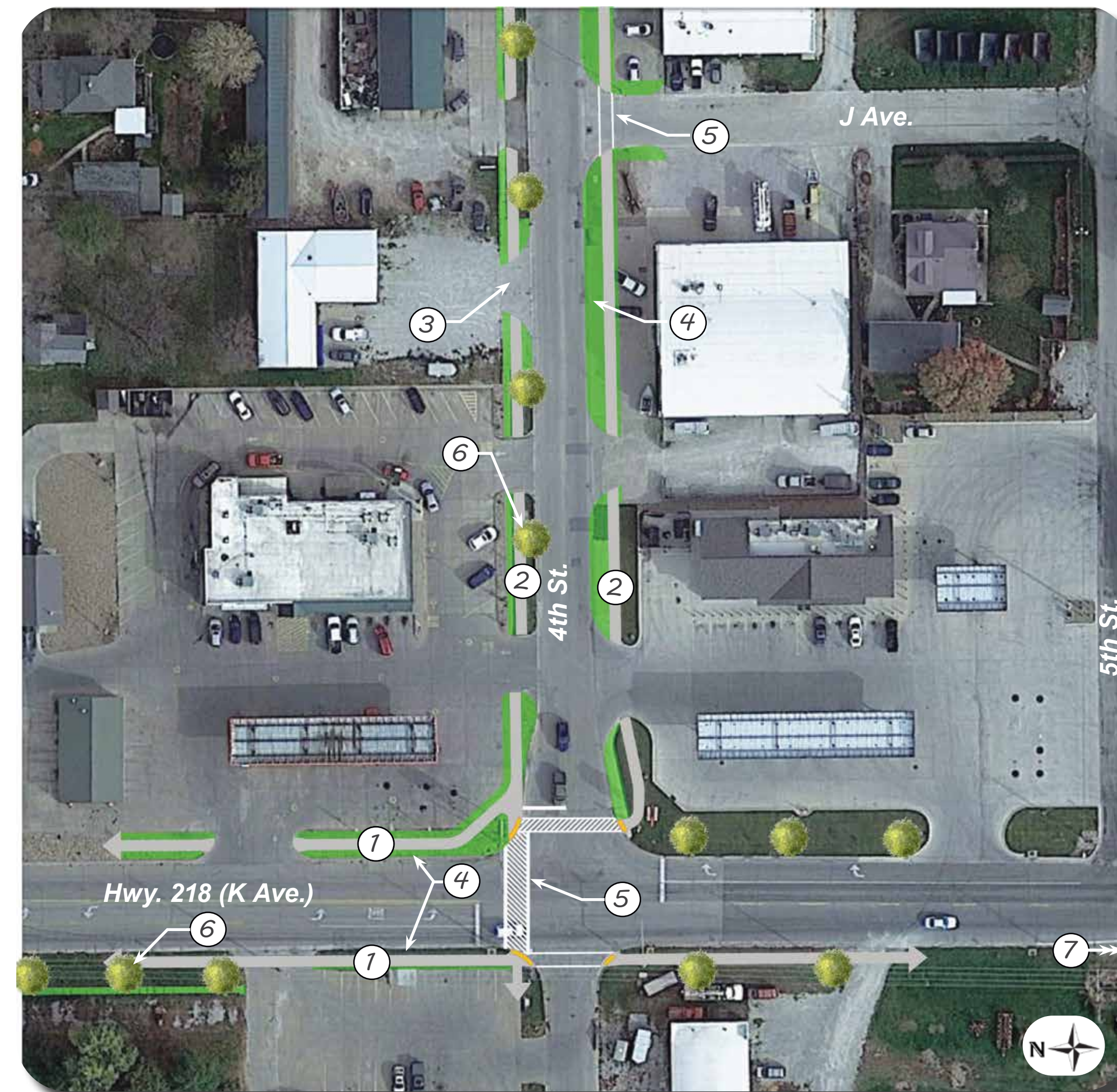
Typical section of Hwy. 218 public corridor (between 1st St. and 4th St.)

### Hwy. 218 Corridor Typical Section Notes

- (A) 6-foot-wide ADA-compliant sidewalk (west side of Hwy. 218: from Hinkle Creek proposed pedestrian bridge crossing north; east side of Hwy. 218: from 4th St. north)
- (B) "Furnishing Zone": Minimum 4-foot-wide green space; location for decorative lighting, way-finding signage, traffic-control signage, possible street tree plantings when width is 6-feet or greater and sight triangles allow
- (C) Existing Hwy. 218 driving lane (+/- 12' to +/- 14')
- (D) Existing Hwy. 218 turning lane (+/- 12' to +/- 14')
- (E) "Frontage Zone" in road right-of-way, width varies - possible street tree plantings when width is 6-feet or greater; buffer plantings to screen views



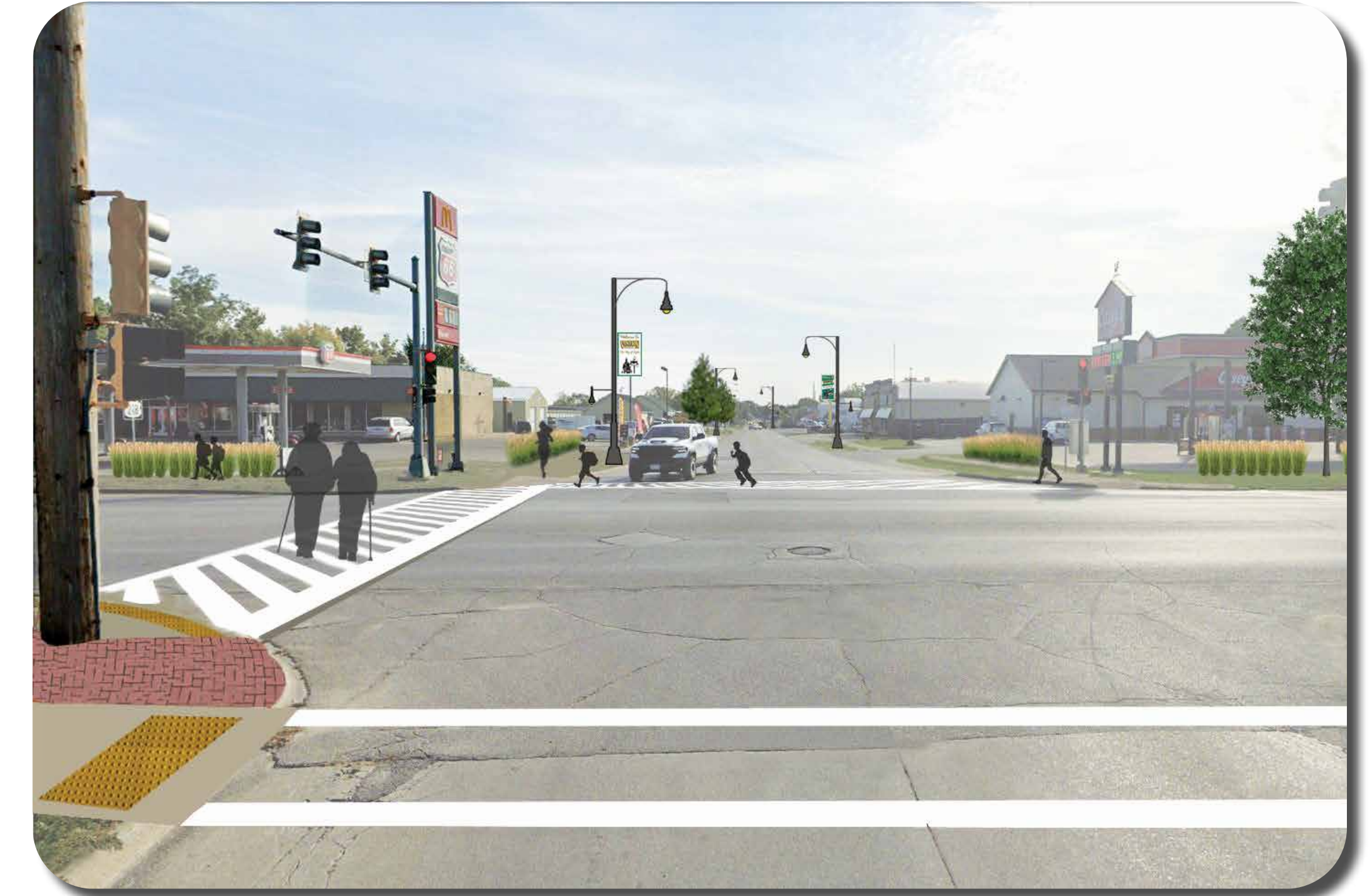
Existing aerial plan view



Aerial plan view with proposed enhancements



Existing Hwy. 218 & 4th St. intersection - photo taken from west side of Hwy. 218 looking easterly along 4th St.



Proposed enhancements - ornamental grass to screen parking lots and highlight intersection; refer to proposed plan view and associated legend on this board for more information on enhancements

### Hwy. 218 and 4th Street Enhancement Highlight

- ① 6-foot-wide ADA-compliant sidewalk along Hwy. 218 north of Hinkle Creek
- ② Minimum 5-foot-wide ADA-compliant sidewalks along primary corridors leading to and from Hwy. 281 (see board 6)
- ③ Reduce size of ingress/egress driveways to standard sizes for specific uses
- ④ Eliminate adjacent connected back-out street parking (except in downtown area)
- ⑤ Paint crosswalks and install associated signage at intersections designated for pedestrian crossings
- ⑥ Install way-finding signage, decorative lighting, and street trees in the right-of-way public green space ("furnishing zone"), as applicable
- ⑦ Install a pedestrian bridge over Hinkle Creek (west side of Hwy. 218)



### Hinkle Creek Crossing at Hwy. 218 (K Ave.)

The lack of a pedestrian bridge over Hinkle Creek along Hwy. 218 was cited by residents as a major transportation barrier affecting the safety of both motorists and pedestrians.

The concept illustrated below was developed based on the feedback received from residents. According to a cursory review of available records, it appears that the Hwy. 218 corridor north of Hinkle Creek may not have a wide enough public right-of-way to accommodate a 10'-wide separated bike path with the necessary clearance zone and grading, but, this would need to be investigated further. It does appear, however, that there is adequate public right-of-way to construct a pedestrian-only bridge (not a recreation trail) that would connect to the proposed pedestrian pavements on either side. A pedestrian-only bridge/crossing would be less expensive than one for a recreational trail because of the decreased travel-way width required.



Existing Hwy. 218 (K Ave.) at Hinkle Creek - photo taken from north side of Hinkle Creek at intersection of 5th Street W. and Hwy. 218 looking southerly along Hwy. 218 toward Hinkle Creek



Proposed concept illustrates the following enhancements: 6-foot-wide ADA-compliant sidewalk, prefabricated pedestrian bridge, branded way-finding signage, decorative pedestrian lighting with banners, right-of-way planted with native prairie grasses and forbs, with wet ditch areas planted with appropriate native wet prairie/wetland vegetation such as prairie cord grass.

### Downtown Connection to Celebration Park (Hwy. 150 & 2nd St.)

Celebration Park is a popular destination for the community given its location along the Cedar River and its close proximity to the downtown and county courthouse. This park hosts the Vinton farmers market is welcoming place to relax, enjoy the river, take a quick walk, or have a picnic.

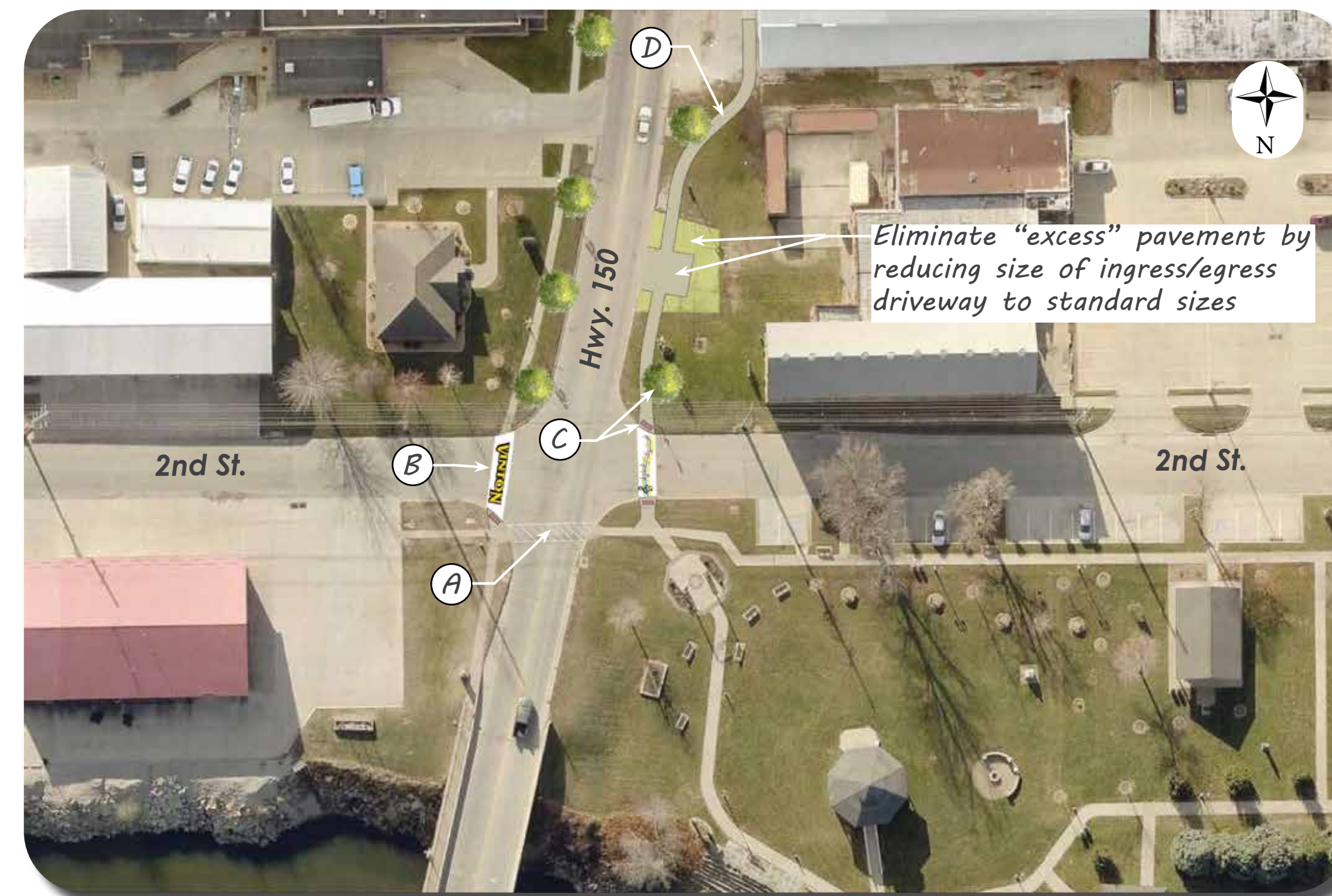
Residents cited concern over the safety of motorists and pedestrians accessing the park from the downtown via the Hwy. 150 corridor. Issues that the proposed concepts address include: traffic calming, traffic control, pedestrian connectivity and ADA accessibility, improved safety, and streetscape aesthetics.

The proposed enhancements are illustrated in both plan and perspective views in the images below and to the right.



Existing aerial plan view

"...[the] only option for us to run is in the streets. There's no sidewalk where [we] could run side by side...a lot of the sidewalks in general are very narrow, so it's hard to just even walk."



Proposed aerial plan view



Existing view of Hwy. 150 & 2nd St. intersection; photo taken from Hwy. 150 bridge looking southerly toward its intersection with 2nd St. toward downtown Vinton (intersection of Hwy. 150/3rd St. and 1st Ave.)



Proposed enhancements noted in highlights and on proposed aerial plan view

#### Downtown Connection to Celebration Park Enhancement Highlights

- (A) High-visibility, painted crosswalk with high-visibility crosswalk warning signs alerting motorists of the crosswalks (i.e. in advance flashing lights)
- (B) Decorative pedestrian crosswalks to enhance safety and aesthetics, as well as create visual connectivity to downtown decorative crosswalks
- (C) Street trees, decorative lighting, way-finding signage, traffic-control devices and ADA-compliant sidewalks assist in traffic calming/control while physically and visually linking Celebration Park to the downtown, creating a more aesthetic gateway, and enhancing the safety and user experience of both the motorist and pedestrian
- (D) ADA-compliant sidewalk on west side of Hwy. 150 corridor to strengthen connectivity and enhance pedestrian safety and accessibility by providing an alternative to route to avoid crossing Hwy. 150 at base of bridge